

11th Street Bridge Protected Bike Lane

Owners:
K.Y.T.C.
City of Covington, KY
City of Newport, KY

Plans Prepared For:
Devou Good Foundation

SHEET INDEX

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|-------|--|
| 1 | TITLE SHEET |
| 2 | TYPICAL SECTIONS |
| 3-4 | PLAN SHEETS |
| | PRODUCT CUT SHEETS AND STANDARD DRAWINGS |



VICINITY MAP

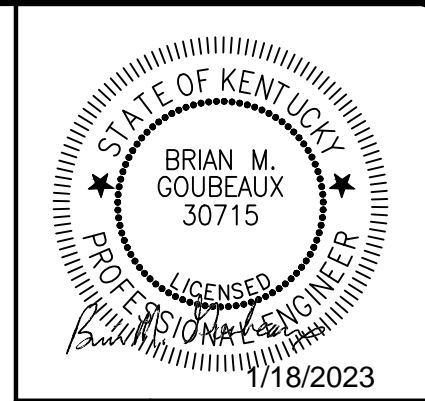
NOT TO SCALE

DESIGN AND GENERAL NOTES

Scope: Installation of a Protected Bike Lane (PBL) in the existing outside, west-bound vehicular lane. All other vehicular lanes are to remain as per existing.

Limitations and Contractor Expectations:

- All utilities are to be cleared prior to any excavation. Contractor is to contact Kentucky 811 and review potential private utilities with the Owner (KYTC) prior to initiating any work.
- Contractor is responsible for all Maintenance of Traffic (MOT) plans, approval and implementation, as per KYTC, City of Covington and City of Newport review and approval.
- Contractor is responsible for means, methods, safety of personnel and temporary support of excavations and nearby structures during construction.
- Any variations or discrepancies between field conditions and the information within this design document are to be immediately brought to the attention of the Engineer.



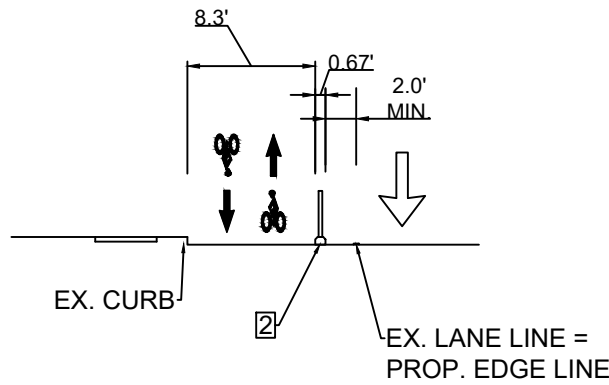
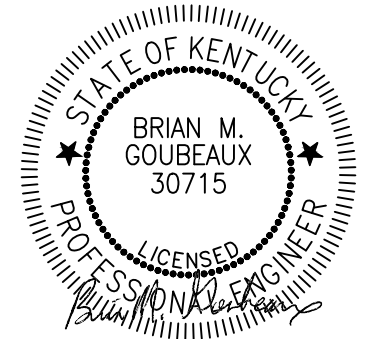
HAMANT ENGINEERING, LLC
 513-383-7040
 hamantengineering@gmail.com

| REVISION DESCRIPTION / DATE: | ISSUE DATE: |
|------------------------------|-------------|
| | 12/14/2022 |

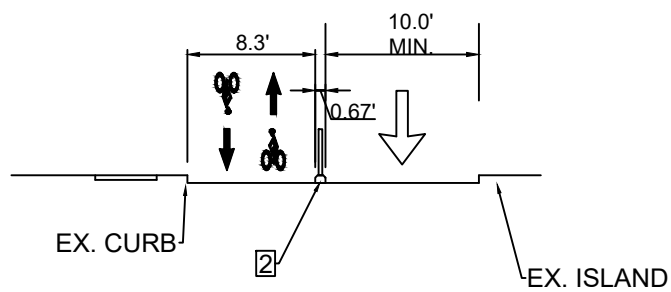
11th Street Bridge Protected Bike Lane
 MLK Jr Dr, Prospect Street - Covington, KY
 11th Street - Newport, KY

TITLE SHEET

Sheet No.: **1/4**

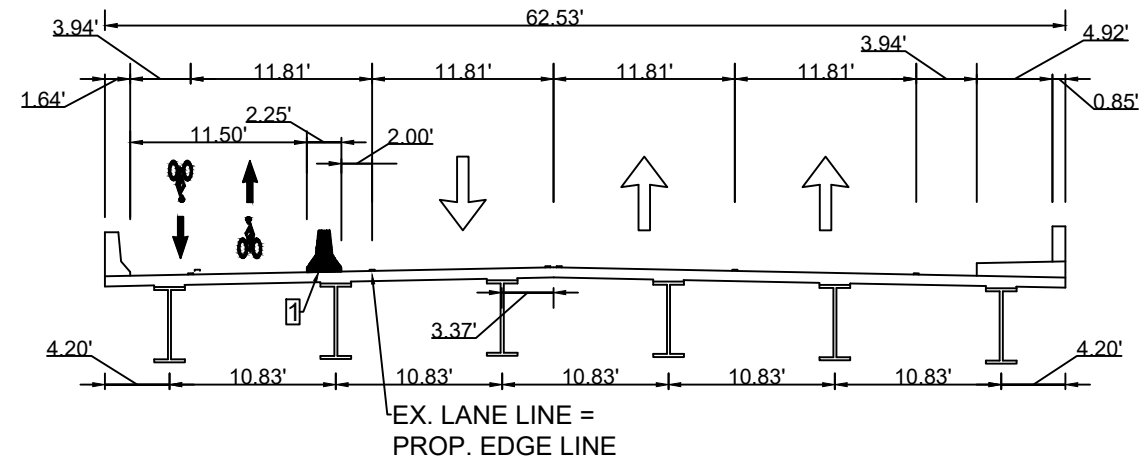


TYPICAL SECTION
PROSPECT AVENUE: BUSH STREET
TO BEGIN BRIDGE APPROACH SLAB

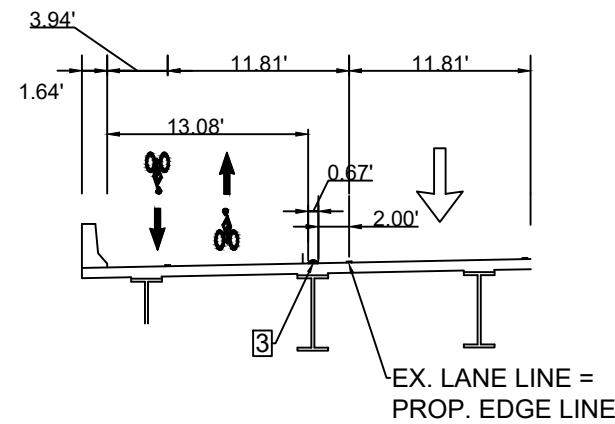


TYPICAL SECTION
PROSPECT AVENUE: 11TH ST
TO BUSH STREET

- LEGEND:
- 1 PRECAST CONCRETE JERSEY BARRIER
 - 2 CONCRETE CURB BARRIER + WHITE POST
 - 3 PEXCO FG 300 w/ WHITE POST



TYPICAL SECTION
BRIDGE: 10' East OF EXPANSION JOINT
TO END OF PROPOSED BARRIER



TYPICAL SECTION
BRIDGE: BEGIN APPROACH SLAB to 10'
East OF EXPANSION JOINT

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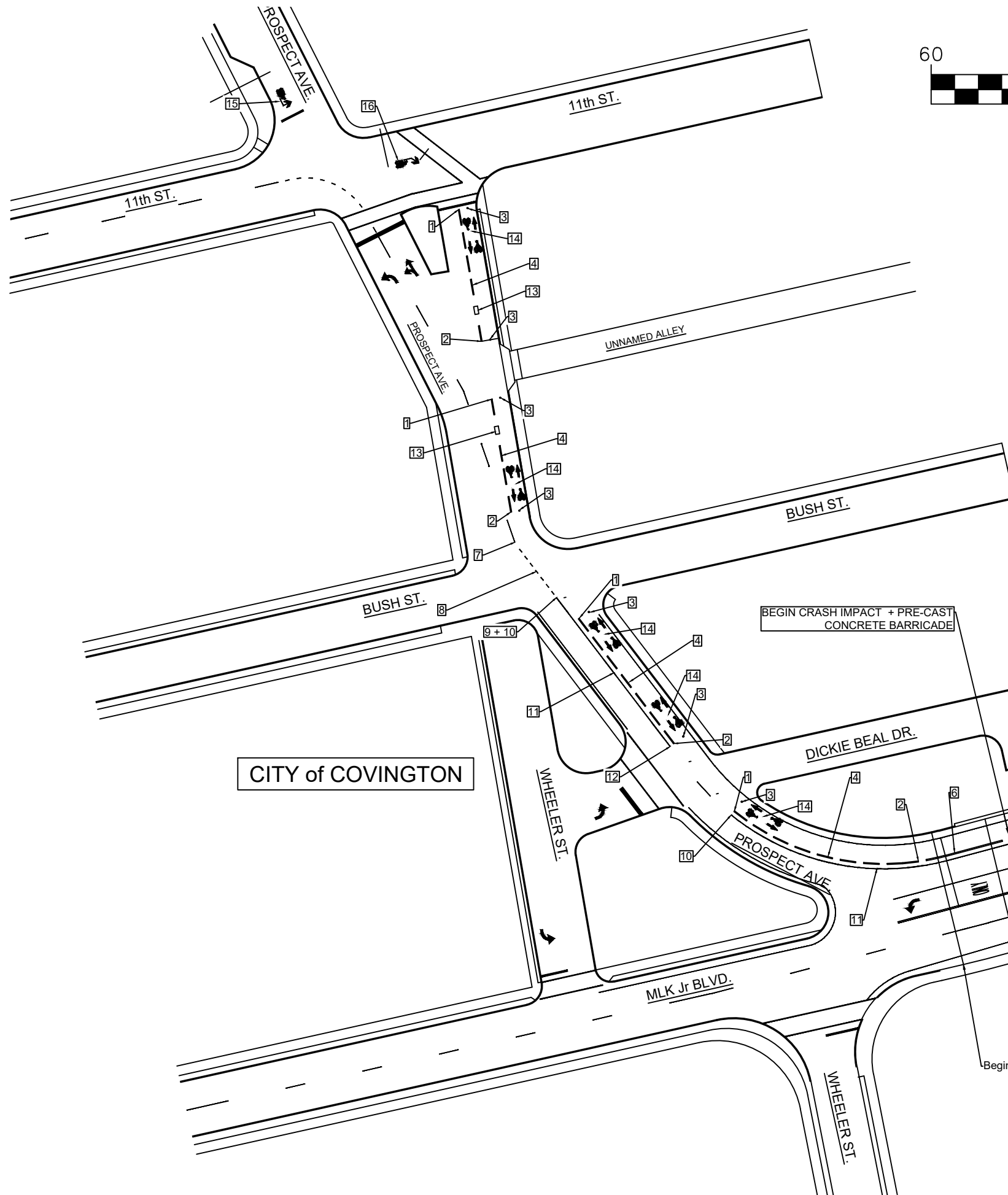
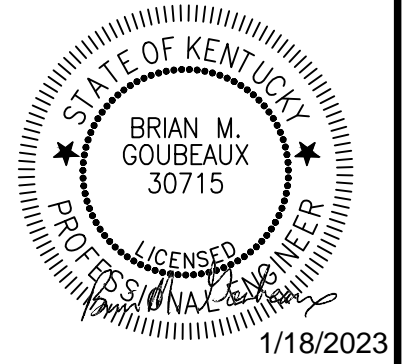
**11th Street Bridge
Protected Bike Lane**

MLK Jr Dr, Prospect Street - Covington, KY
11th Street - Newport, KY

Typical Sections



(IN FEET)
 1 inch = 60 ft. Full Size
 1 inch = 120 ft. Half Size



- 1 = BEGIN CONCRETE CURB/POST
- 2 = END CONCRETE CURB/POST
- 3 = DEVELOTECH, PEDZONE POST, CUSTOM, WITH "DO NOT ENTER" SYMBOL & "EXCEPT BIKES" TEXT
- 4 = PRECAST CONCRETE CURB BARRIER, WITH CYCLO-ZONE POST ON TOP OBJECT MARKER POST ON TOP (PZ-2120, WA-33L)
- 5 = PRECAST CONCRETE JERSEY BARRIER
- 6 = PEXCO FG 300 (WHITE) w/ POST (WHITE)
- 7 = BEGIN LANE LINE EXTENSION
- 8 = NEW PAVEMENT MARKING: DASHED, 4" DOTTED WHITE LANE LINE EXTENSION. LENGTH = 2', WITH 2'-6" GAP SPACING.
- 9 = END LANE LINE EXTENSION
- 10 = BEGIN EDGE LINE
- 11 = NEW PAVEMENT MARKING: SOLID, WHITE, 4" EDGE LINE.
- 12 = END LANE LINE EXTENSION
- 13 = PLANTER, PLANTERCRAFT, 48"x24"x24"HT
- 14 = NEW PAVEMENT MARKING: BIKE LANE, 2-WAY, WHITE
- 15 = NEW PAVEMENT MARKING: DIRECTIONAL SHARROW, LEFT TURN, WHITE
- 16 = NEW PAVEMENT MARKING: DIRECTIONAL SHARROW, VEER RIGHT, WHITE

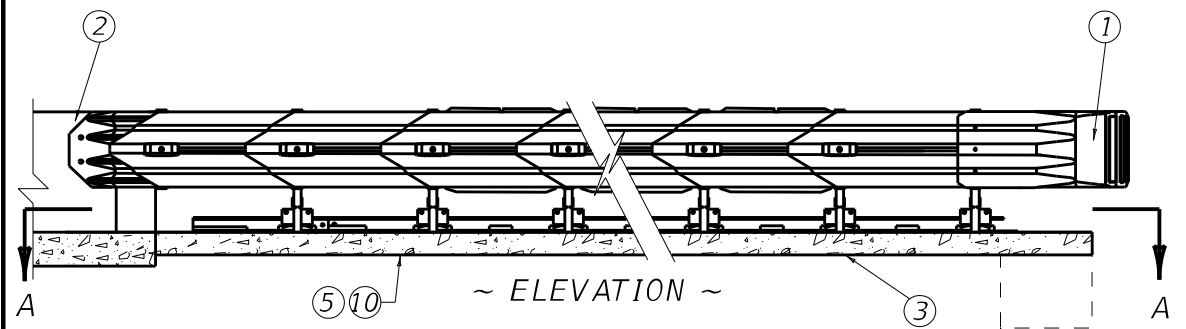
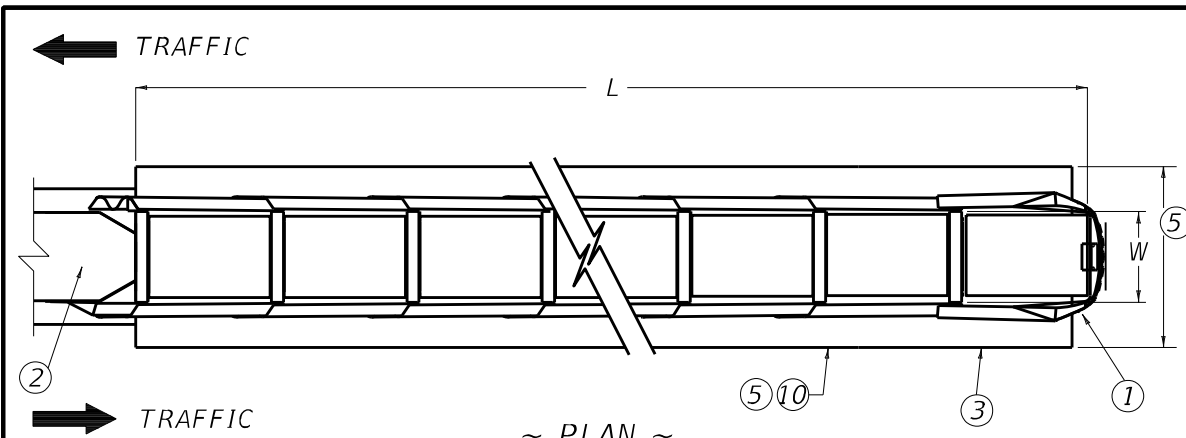
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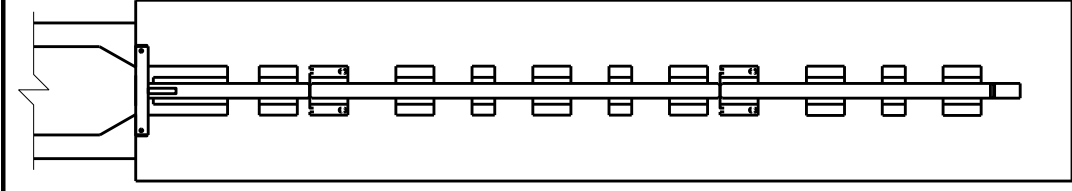
11th Street Bridge Protected Bike Lane
 MLK Jr Dr, Prospect Street - Covington, KY
 11th Street - Newport, KY

Plan Sheet

Sheet No.: **3/4**



Additional Concrete Pad Construction may be Required Per Manufacturer Specifications



~ SECTION A-A ~
Reinforcement Per Manufacturer Specifications

~ CONCRETE PAD SECTION ~
(Per Manufacturer Specifications)
Refer to Notes ⑤

~ NOTES ~

- ① NOSE ASSEMBLY (OBJECT MARKER TYPE 1 AS REQUIRED)
- ② BACKUP
- ③ 6" CONCRETE PAD PER MANUFACTURER SPECIFICATIONS.
- 4. CRASH CUSHION TYPE VI, CLASS A, B, C
 - A CLASS B OR C, AS REQUIRED
 - B EITHER TEST LEVEL 2 (TL2) OR TEST LEVEL 3 (TL3), AS REQUIRED.
 - C SEE "CONNECTION DETAILS OF CRASH CUSHION TYPE VI TO DOUBLE FACE GUARDRAIL".
- ⑤ THE CONCRETE PAD, PAD EXCAVATION AND STEEL REINFORCEMENT, INSTALLED IN PLACE SHALL BE INCLUDED IN THE UNIT PRICE BID FOR CRASH CUSHION TYPE VI. DIMENSION AND REINFORCEMENT SPECIFICATIONS FOR CONCRETE PADS ARE TO BE PROVIDED BY THE MANUFACTURER. THE PAD WILL NOT BE REQUIRED WHEN THE UNIT IS CONSTRUCTED ON RIGID PAVEMENT.
- 6. CRASH CUSHION TYPE VI MAY BE USED AT THE END OF: CONCRETE MEDIAN BARRIER, BRIDGE PIERS AND STEEL "W" BEAM GUARDRAIL (DOUBLE FACE).
- 7. WHEN CRASH CUSHION TYPE VI CONNECTS TO: CONCRETE MEDIAN BARRIER OR BRIDGE PIER THE CONTRACT UNIT PRICE SHALL INCLUDE: CRASH CUSHION TYPE VI, ALL HARDWARE, ADDITIONAL RAIL ELEMENTS, POST, CONCRETE PAD AND ALL OTHER INCIDENTALS NECESSARY TO COMPLETE THE INSTALLATION.
- 8. THIS DRAWING DEPICTS A CONNECTION OF CRASH CUSHION TYPE VI TO CONCRETE MEDIAN BARRIER END. FOR THIS APPLICATION SEE CURRENT STD. DWG. [RBE-065](#) "CONCRETE MEDIAN BARRIER END".
- 9. WHEN CRASH CUSHION TYPE VI CONNECTS TO DOUBLE FACE GUARDRAIL SEE CURRENT SEPIA 018 "CONNECTION DETAILS OF CRASH CUSHION TYPE VI TO DOUBLE FACE GUARDRAIL".
- ⑩ CRASH CUSHIONS ARE TO BE INSTALLED PER MANUFACTURER SPECIFICATIONS, INCLUDING THE CONCRETE PAD.
THE MANUFACTURER SHALL FURNISH TWO (2) SETS OF SHOP PLANS TO THE CONTRACTOR WITH EACH INSTALLATION.
- 11. THE CRASH CUSHION TYPE VI MAY ALSO BE UTILIZED FOR TEMPORARY USE AND CONSTRUCTION ZONES (CLASS BT).
- 12. A CRASH CUSHION TYPE VI CLASS B IS TO BE USED IN AREAS WHERE CRASH HISTORY IS NOT KNOWN TO BE SEVERE.
- 13. A CRASH CUSHION TYPE VI CLASS C IS CONSIDERED A SEVERE USE CRASH CUSHION.

ADDITIONAL BID ITEMS AND UNIT TO BID
OBJECT MARKER TYPE 1 (AS REQUIRED) EACH

| CLASS | SPEED (MPH) | ATTENUATOR | | | SUGGESTED ADT* RANGE (P.C.P.L.) ** |
|-------|---------------|------------|---------------------|--------|------------------------------------|
| | | MODEL | PRODUCT NAME | LENGTH | |
| B | 45 & LESS | TL2 | UNIVERSAL TAU-M | 14'-2" | UP TO 12,000 |
| | | | 3-BAY QUADGUARD M10 | 12'-0" | |
| | OVER 45 | TL3 | UNIVERSAL TAU-M | 22'-9" | |
| | | | 5-BAY QUADGUARD M10 | 18'-0" | |
| C | OVER 45 | TL3 | SCI100GM | 23'-0" | 8,000 AND OVER |
| | | | QUADGUARD ELITE M10 | 26'-7" | |

W = 2'-0" (INSIDE BAY WIDTH)


* AVERAGE DAILY TRAFFIC
** PASSENGER CARS PER LANE


USE WITH CUR. STD. DWG.
[RBE-065](#) [RBC-110](#)

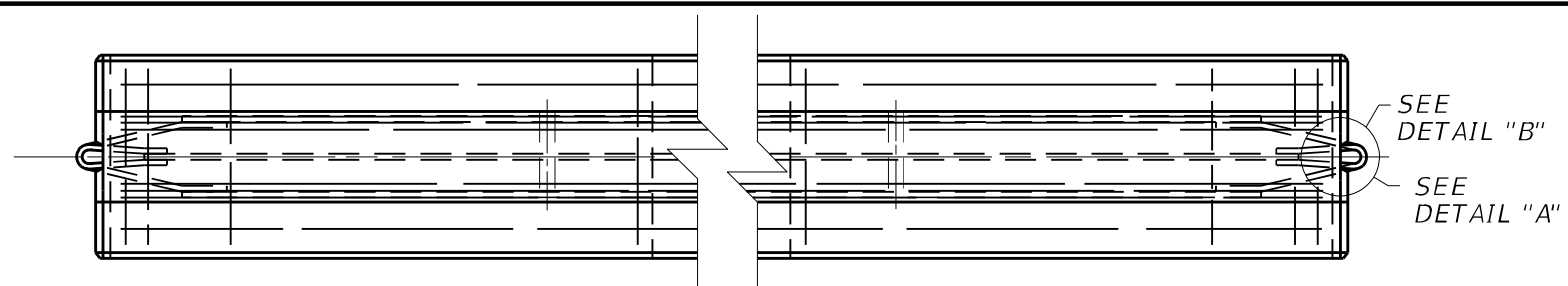
KENTUCKY
DEPARTMENT OF HIGHWAYS

CRASH CUSHION
TYPE VI A B C
(ONE & TWO DIRECTION)

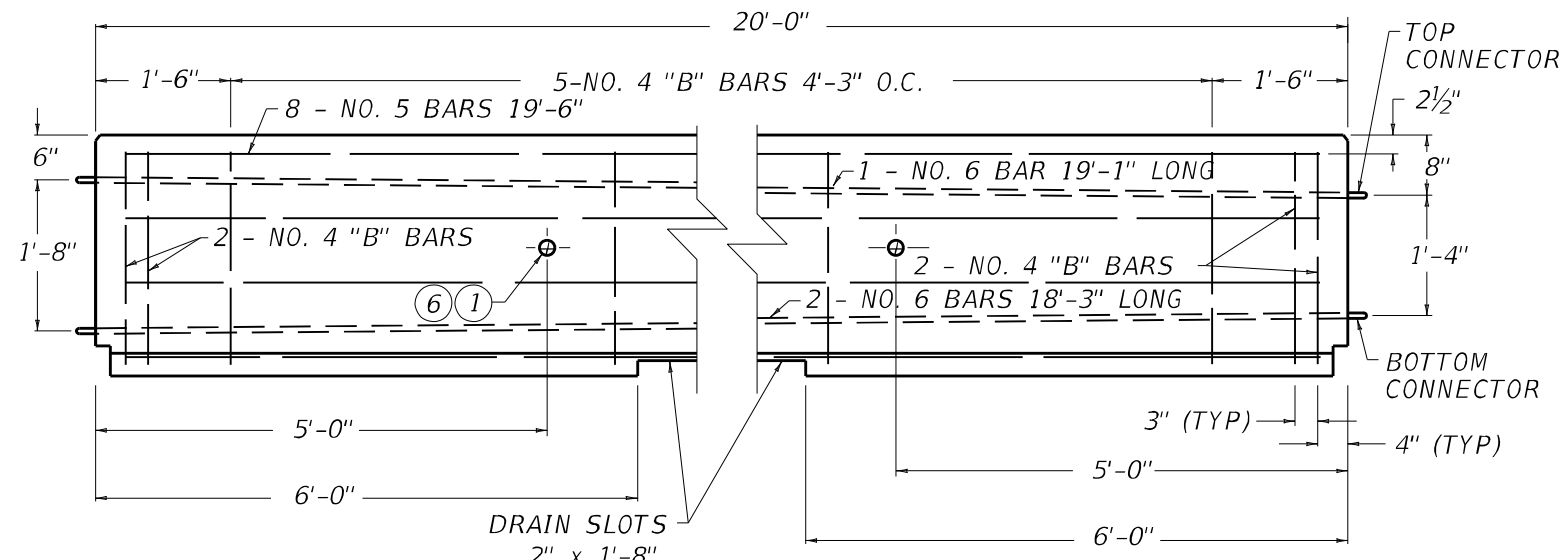
STANDARD DRAWING NO. [RBE-060-15](#)

SUBMITTED  02-26-20
DIRECTOR DIVISION OF DESIGN DATE

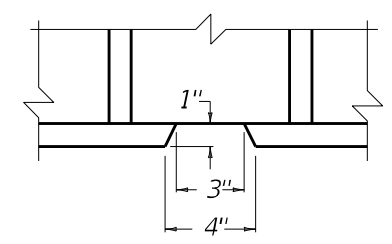
APPROVED  02-26-20
SEAL PROFESSIONAL ENGINEER DATE



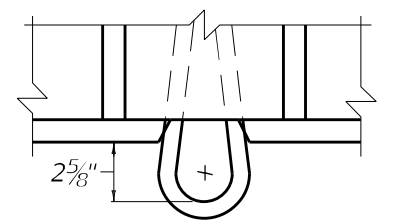
PLAN VIEW



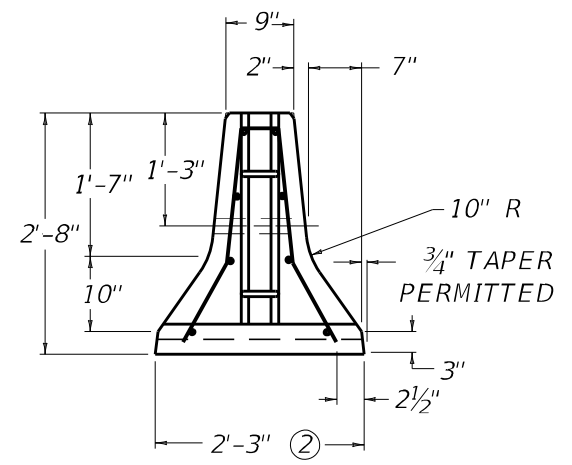
ELEVATION VIEW



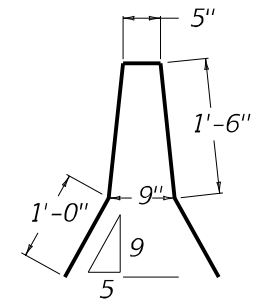
DETAIL "A"



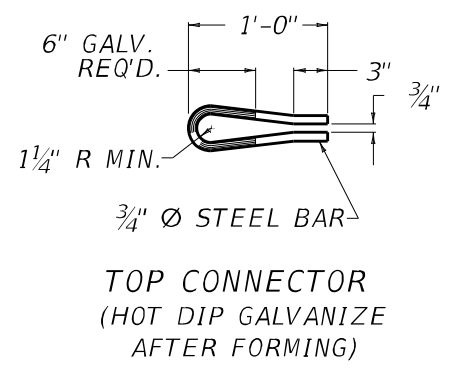
DETAIL "B"



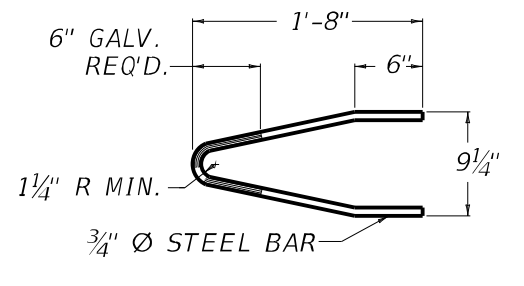
RIGHT ELEVATION VIEW



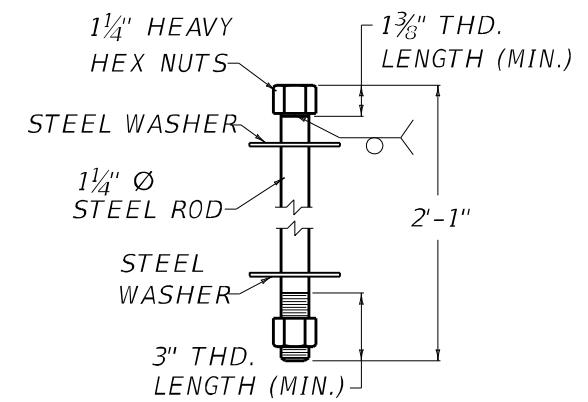
DETAIL OF "B" BAR



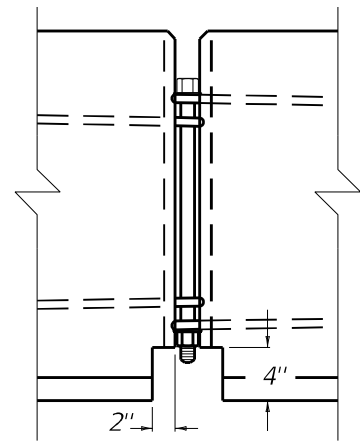
TOP CONNECTOR
(HOT DIP GALVANIZE AFTER FORMING)



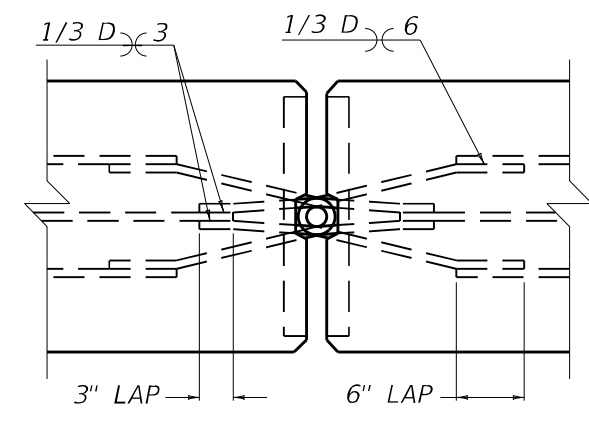
BOTTOM CONNECTOR
(HOT DIP GALVANIZE AFTER FORMING)



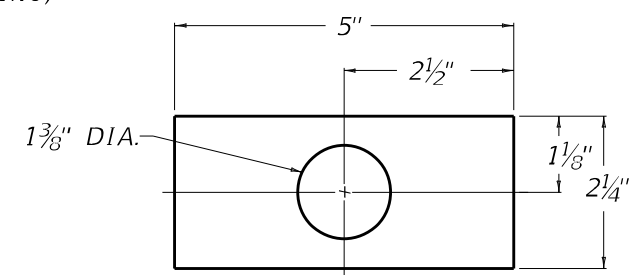
CONNECTOR PIN



ELEVATION OF CONNECTOR DETAIL



PLAN OF CONNECTION DETAIL



1/2" THICK STEEL WASHER

~ NOTES ~

- BID ITEM AND UNIT TO BID
CONCRETE BARRIER WALL TYPE 9T LF
- (1) 2" DIA. LIFTING HOLE - 2 REQUIRED FOR EACH SECTION. FORMED WITH 2" P.V.C. PIPE OR EQUAL.
- (2) TAPER NOT INCLUDED IN BASE WIDTH.
- 3. SHOP DRAWINGS SHALL BE APPROVED PRIOR TO MANUFACTURE.
- (4) BASED ON 150 LBS./CU. FT.
- 5. PLACE ALL STEEL REINFORCEMENT A CLEAR DISTANCE OF 2" MIN. FROM OUTSIDE FACE OF WALL, EXCEPT WHERE SHOWN OTHERWISE.
- (6) LIFTING BARS SHALL BE REQUIRED TO PREVENT SPALLING OF CONCRETE AROUND HOLES.
- 7. PREVIOUS WALL MANUFACTURED ACCORDING TO STANDARD DRAWING RBM-115 MAY STILL BE USED. ANY NEW BARRIER WALL TYPE 9T MANUFACTURED SHALL COMPLY TO THIS STANDARD DRAWING.
- 8. A PERMISSABLE ALTERNATE FOR THE PIN AND LOOP CONNECTOR IS JJ HOOK MANUFACTURED BY EASI-SET INDUSTRIES OUT OF MIDLAND, VA. SEE MANUFACTURER'S SHOP DRAWINGS FOR DETAILS ON JJ HOOK CONNECTOR AND RECOMMENDED REINFORCEMENT. THE BARRIER WALL'S DIMENSIONS, SHAPE, LENGTH AND THE DRAIN SLOT DIMENSIONS AND LOCATIONS SHALL MATCH THIS DRAWINGS CURRENT DIMENSIONS. (1) (6)

APPROXIMATE QUANTITIES

| 20' | | |
|--------|-------------|------------|
| REINF. | CONC. | WEIGHT (4) |
| LBS. | CU. YD./FT. | TONS |
| 195 | 0.12 | 5.0 |

USE WITH CUR. STD. DWG.
RBM-120

KENTUCKY
DEPARTMENT OF HIGHWAYS

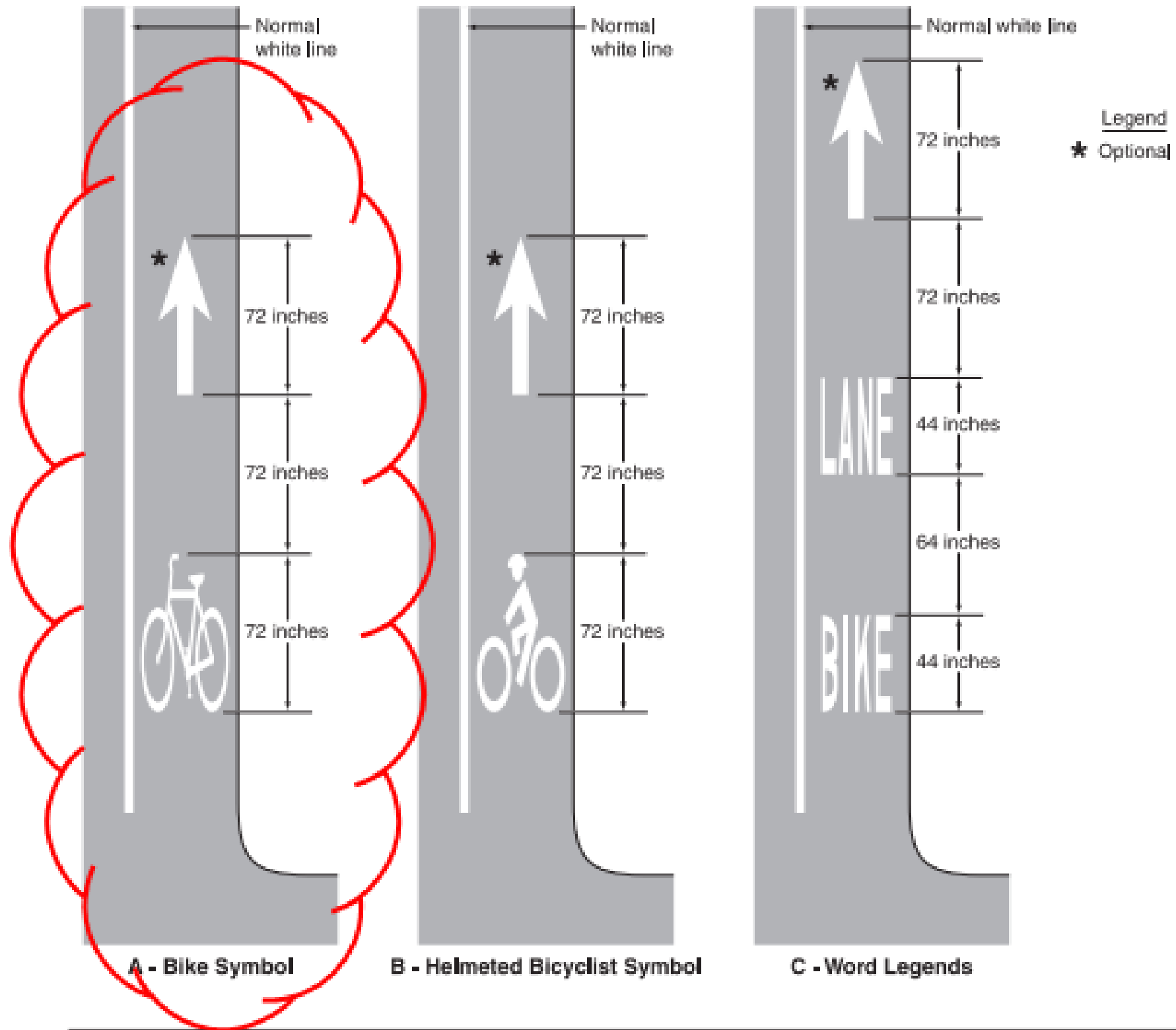
CONCRETE BARRIER
WALL TYPE 9T
(TEMPORARY)

STANDARD DRAWING NO. RBM-115-10

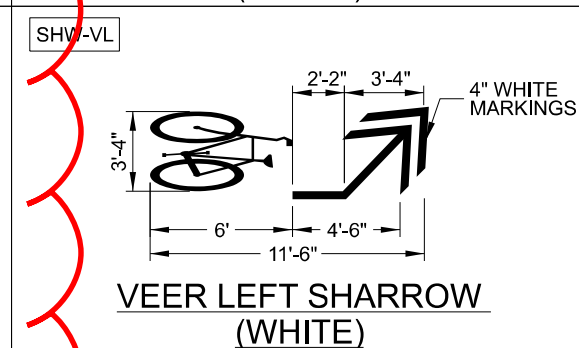
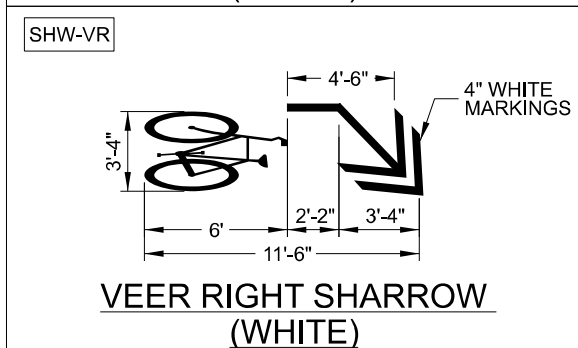
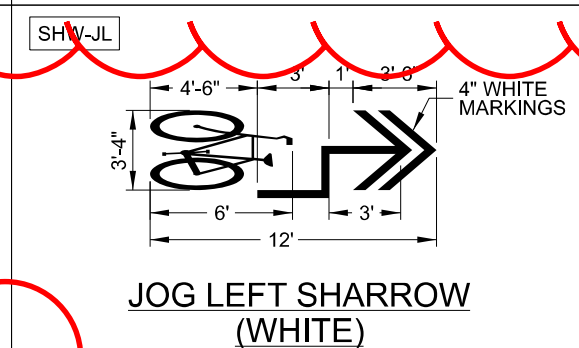
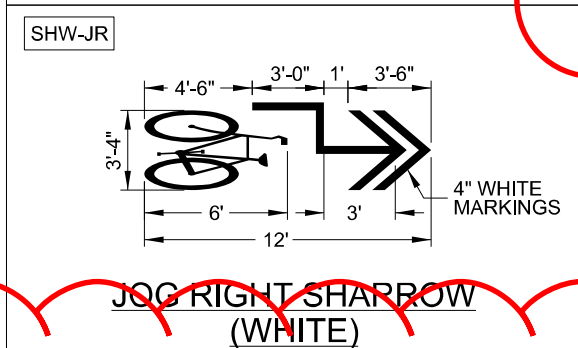
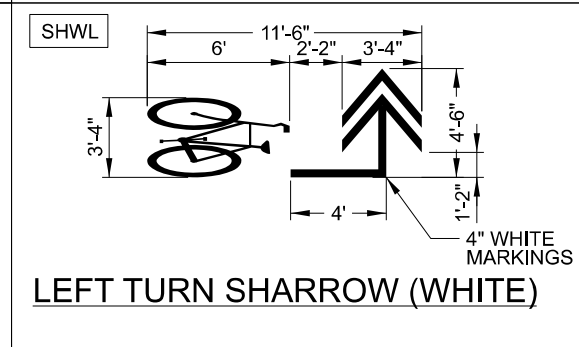
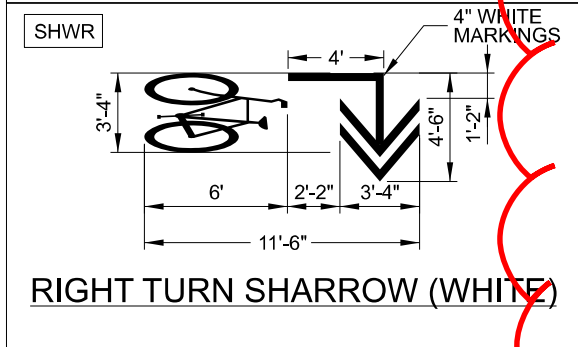
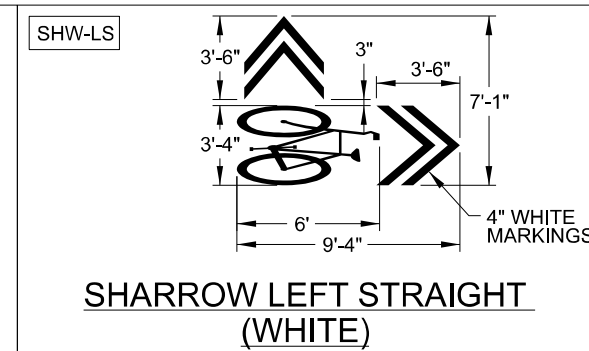
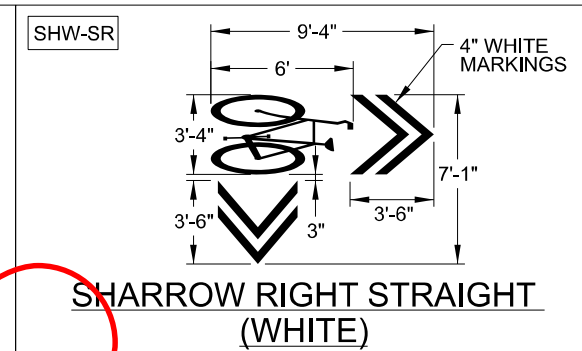
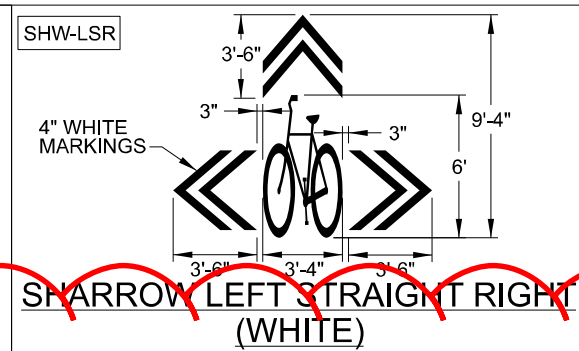
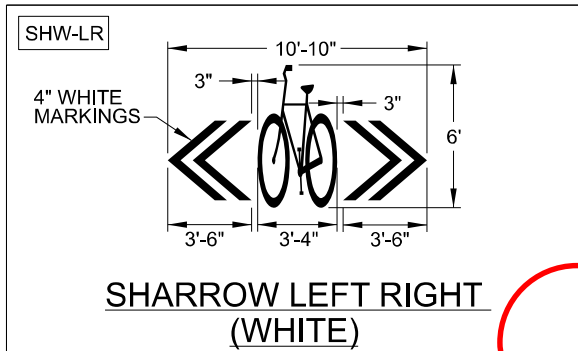
SUBMITTED *William S. Hall* 12-01-15
DATE

APPROVED *Shelby* 12-01-15
STATE HIGHWAY ENGINEER DATE

Figure 9C-3. Word, Symbol, and Arrow Pavement Markings for Bicycle Lanes



Plot Date: 9/12/2022 11:56:23 AM
 Filename: U:\ustation\DCN\CITY OF PORTLAND - STANDARD DRAWINGS\SECTION DIVISIONS\3-STREETS\MICROSTATION\STD DRAWING NO P-437A.PLOT_SHT_P-437.dgn
 SCALE: 1" = 1'-0"



| | | |
|--|--|--------------------------------------|
| The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user. | PBOT PORTLAND BUREAU OF TRANSPORTATION City Engineer | |
| | Standard Drawing Title PAVEMENT MARKINGS STANDARD DETAIL BLOCKS SHARROWS | |
| | Effective Date: Calc. Book No.: Std. Drg. Report Date: | Standard Drawing No. P-437 |
| Note: All material and workmanship shall be in accordance with the City of Portland Standard Construction Specifications. | | |

QuadGuard® M10

CRASH CUSHIONS



MASH

QuadGuard® M10

The QuadGuard® M10 is a redirective, non-gating crash cushion that consists of an engineered steel nose and crushable, energy absorbing cartridges surrounded by a framework of steel Quad-Beam™ panels. The system is tested to the Manual for Assessing Safety Hardware (MASH) Test Level 3 and Test Level 2. It can be used to shield fixed objects up to 24" (610 mm). A wide Test Level 3 system, the QuadGuard M Wide, is also available for fixed objects up to 69" (1.75 m).

The QuadGuard® M10, as a member of the QuadGuard® family of crash cushions, consists of many of the same components as the original Test Level 3 QuadGuard® platform and framework in addition to an engineered steel nose and monorail shims.

FEATURES

- Self-supporting steel nose.
- Tension strut backup.
- Monorail guide stabilizers.
- Anchorage in concrete.
- High strength Quad-Beam™ panels.
- Does not use anchoring chains or tension cables

ASSEMBLY AND MAINTENANCE

- Damaged cartridges are replaceable.
- Potentially reusable after an impact within MASH crash test standards.*

SPECIFICATIONS

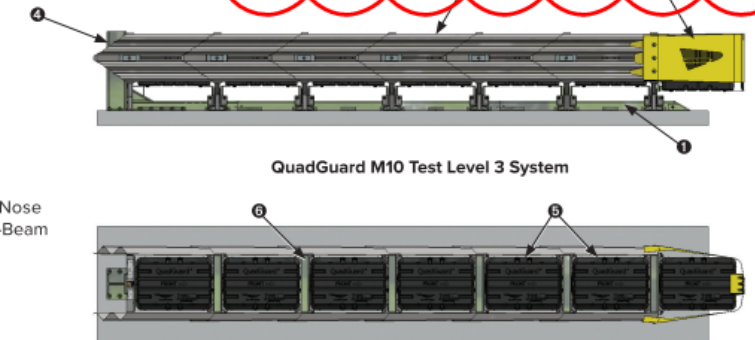
- QuadGuard M10 Test Level 3 System**
- System Length – 22' 0" (6.71 m)
 - System Width – 24" (610 mm)

- QuadGuard M Wide Test Level 3 System (not shown)**

- System Length – 22' 0" (6.71 m)
- System Width – 69" (1.75 m)

- QuadGuard M10 Test Level 2 System**

- System Length – 13' 0" (3.96 m)
- System Width – 24" (610 mm)



- 1 Monorail Base
- 2 Engineered Steel Nose
- 3 Telescoping Quad-Beam Fender Panel
- 4 Backup
- 5 Cartridge
- 6 Diaphragm

DISTRIBUTED BY

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 1.888.323.6374

UNIVERSAL TAU-M™

REDIRECTIVE, NON-GATING CRASH CUSHION

- PARTIALLY REUSABLE DESIGN
- QUICK AND EASY TO INSTALL AND REPAIR
- MASH TL-3 / TL-2 TESTED

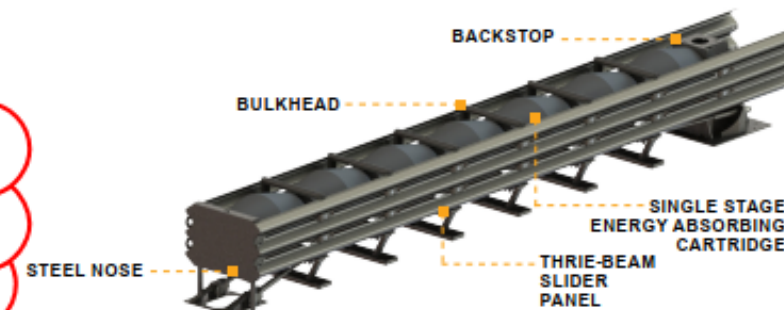


BARRIER SYSTEMS™
BY LINDSAY

UNIVERSAL TAU-M™ | REDIRECTIVE, NON-GATING CRASH CUSHION

PHYSICAL SPECIFICATIONS

| MASH Classification | TL-3 / TL-2 | |
|---------------------|-------------|--------|
| TL-3 Length | 22'0" | 6.71 m |
| TL-2 Length | 14' 2" | 4.33 m |
| Width | 30" | 762 mm |
| Height | 32 s" | 830 mm |



MASH TL-3 AND TL-2 TESTED

The Redirective, Non-Gating, Partially Reusable, Universal TAU-M Crash Cushion (TAU-M) is designed to meet MASH TL-3 and TL-2 testing requirements in a compact, partially reusable design. The system is available for concrete and asphalt applications and can shield hazards with widths up to 30" (880 mm). Ease of installation, fast on-site repairs, and numerous transition options make the TAU-M system an ideal crash cushion to shield most roadside and median hazards. The TAU-M system is also available as an upgrade kit to retrofit existing NCHRP 350 TAU-II Systems into MASH TAU-M Systems.

FREQUENTLY ASKED QUESTIONS

What components of the TAU-M System need to be replaced after a design impact?

Typically, only the damaged cartridges will need to be replaced. The nose and slider panels are designed to withstand multiple design impacts.

What type of foundation is needed for the TAU-M System?

A 6" (152 mm) reinforced concrete pad is required. The Universal TAU-M System can also be installed in as little as 4" (102 mm) of asphalt.

What transitions are available?

Since TAU-M transitions are non-proprietary, all MASH approved thrie-beam barrier transitions will work with the system.

What are the major differences between the NCHRP 350 TAU-II System and the MASH TAU-M System?

Upgrades to the system include heavy gauge steel slider panels, large diameter cables, redesigned compact backstop, shortened length, and single stage energy absorbing cartridges.

Can the NCHRP 350 TAU-II System be retrofitted to a MASH TAU-M System?

Yes, existing NCHRP 350 TAU-II Systems can be retrofitted into MASH TAU-M Systems.

FEATURES

- Minimum number of anchors needed to secure the system
- Reduced length vs. NCHRP 350 TL-3 system
- Upgraded slider panels for increased durability
- Can be installed over bridge expansion joints
- Concrete and asphalt systems available
- Numerous non-proprietary transition options
- NCHRP 350 to MASH Retrofit kits available

DISTRIBUTED BY:



18135 Burke Street, Suite 100 • Omaha, NE 68022 • +1 (402) 829-6800 U.S. Toll Free: (888) 800-3691 • www.lindsay.com

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PT # TAUM01-10202018

SPECIFICATION - FLEXIBLE BOLLARD WITH OBJECT MARKER

The flexible bollards shall be Cyclo-Zone flexible bollard with Object marker (by Develotech Inc.). Bollards are black with yellow reflective sheeting with object marker screen printed on both sides, as per the installation drawings provided per project.

Bollards must comply with the following specifications and shop drawing attached.

- Height: 122 cm
- Width: 10.2 cm
- Thickness: 0.3 cm
- Width of the Reflective Sheeting: 76 mm
- Height of the Reflective Sheeting: 91.4 mm
- Object marker Reflective Sheeting fixed on both sides

Installation shall be in accordance with manufacturer’s recommendations, as follows:

ASPHALT PAVEMENT INSTALLATION

Drill four holes (Diameter: 3/4 inch, Depth: 4 inches), pour in Bitu-Link™ epoxy, insert Cyclo-Zone anchors and wait 24 hours until the epoxy has hardened. Then, fill the anchors with white grease and fasten the bollard with four bolts.



CONCRETE ROADWAY OR CURB INSTALLATION

Drill four holes (Diameter: 1/2 inch, Depth: 1 3/4 inch), insert drop-in anchors and hammer them with a punch. Then, fill the anchors with white grease and fasten the bollard with four bolts.



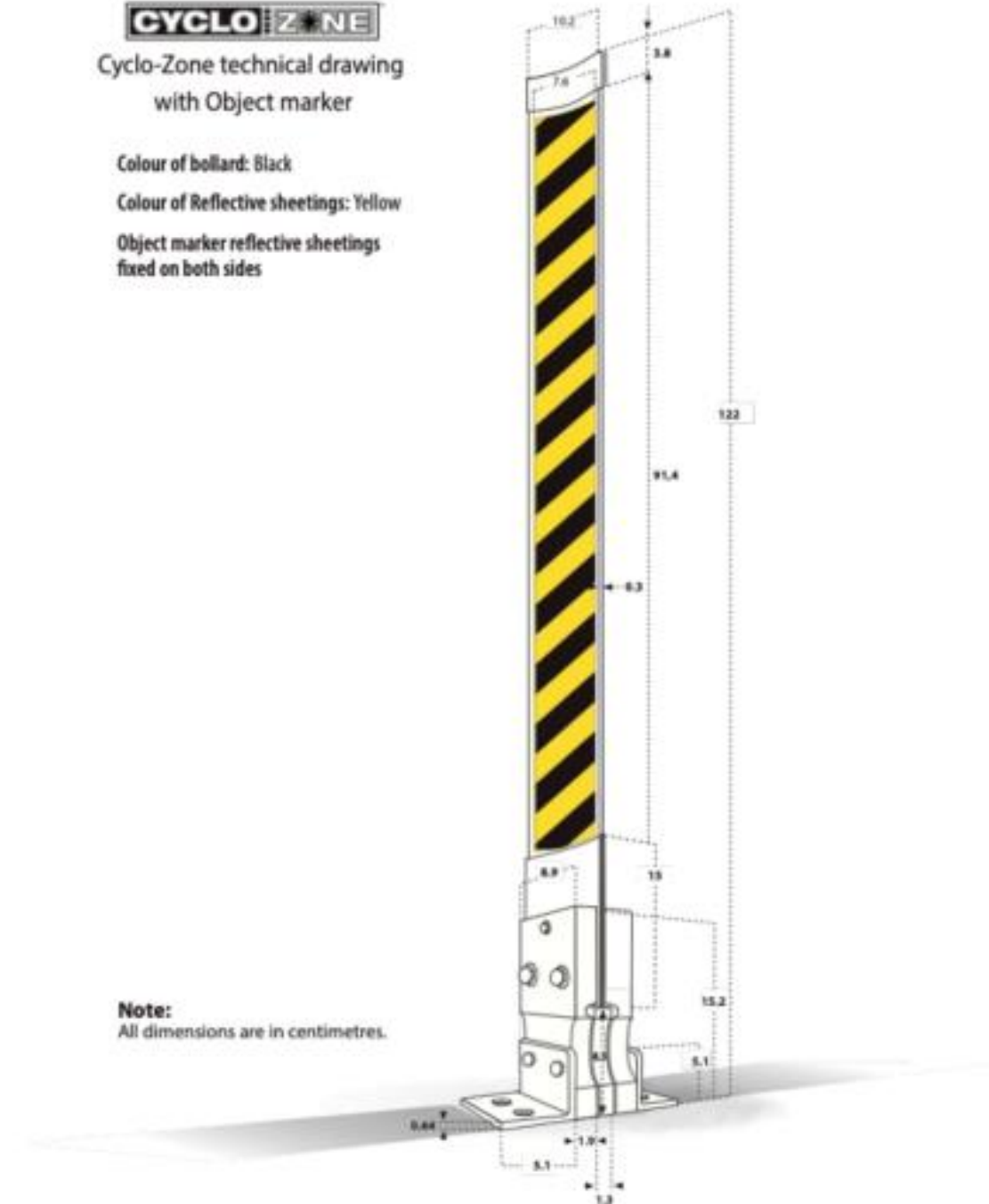
CYCLO-ZONE

Cyclo-Zone technical drawing with Object marker

Colour of bollard: Black

Colour of Reflective sheetings: Yellow

Object marker reflective sheetings fixed on both sides



SPECIFICATION - FLEXIBLE SIGN

The In-Road flexible signs shall be Ped-Zone sign (by Develotech Inc.). The signs must have white reflective sheeting with Do not enter - except bicycles.pdf graphic on the front side and yellow reflective sheeting with PZ-2120 (WA-33L).pdf graphic on the back side, as per the installation drawings provided per project.

Signs must comply with the following specifications, shop drawing and graphics attached.

- Height: 122 cm
- Width: 32.4 cm
- Thickness: 0.3 cm
- Width of the Reflective Sheeting: 30.5 cm
- Height of the Reflective Sheeting : 94 cm
- Reflective Sheeting fixed on both sides
- Anti Graffiti Protection Film on both sides

Installation shall be in accordance with manufacturer's recommendations, as follows:

ASPHALT PAVEMENT INSTALLATION

Drill four holes (Diameter: 3/4 inch, Depth: 4 inches), pour in Bitu-Link™ epoxy, insert Ped-Zone anchors and wait 24 hours until the epoxy has hardened. Then, fill the anchors with white grease and fasten the bollard with four bolts.

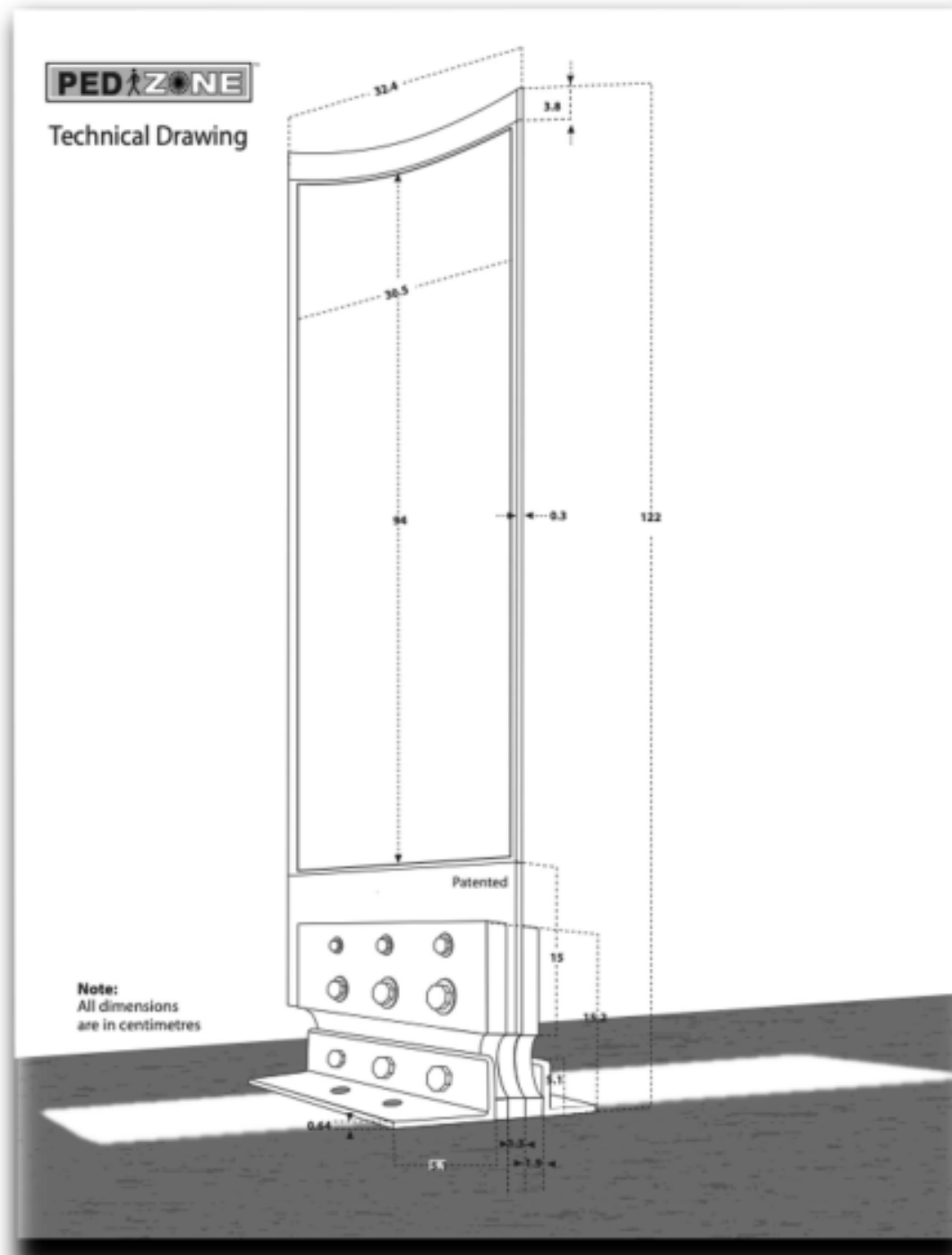


CONCRETE ROADWAY OR CURB INSTALLATION

Drill four holes (Diameter: 1/2 inch, Depth: 1 3/4 inch), insert drop-in anchors and hammer them with a punch. Then, fill the anchors with white grease and fasten the bollard with four bolts.

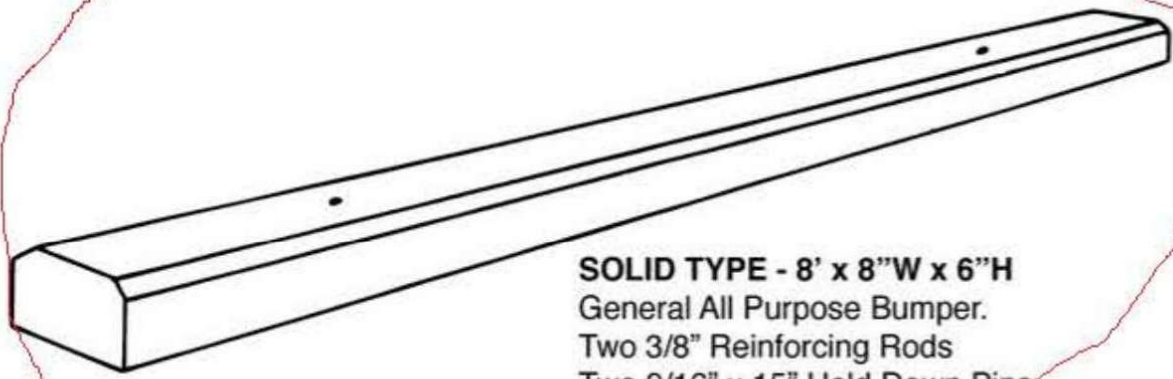


Develotech Inc. 4655, Tanguay Street Saint-Hyacinthe (Quebec) J2R 2H3 Canada
Phone: 450 799-4537 Fax: 450 799-4538 sales@develotech.com

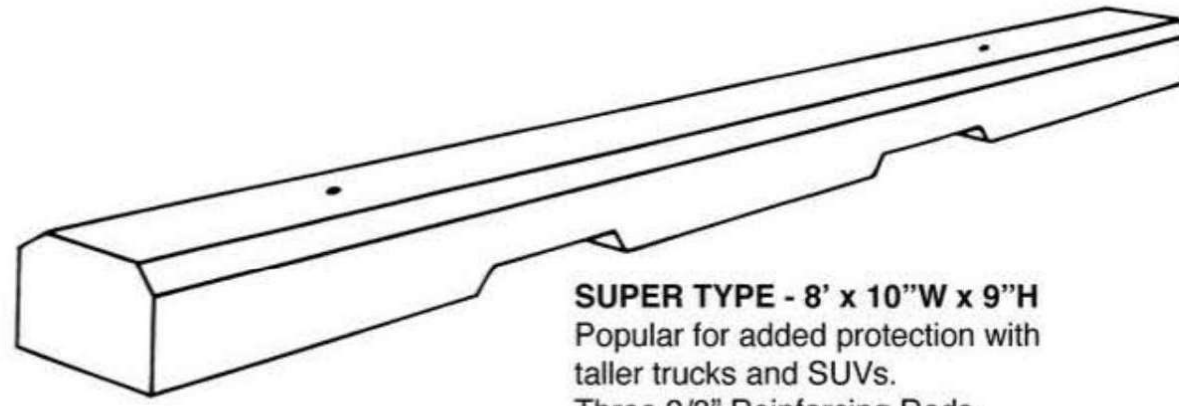


Do not enter - except bicycles.pdf | PZ-2120 (WA-33L).pdf graphic:

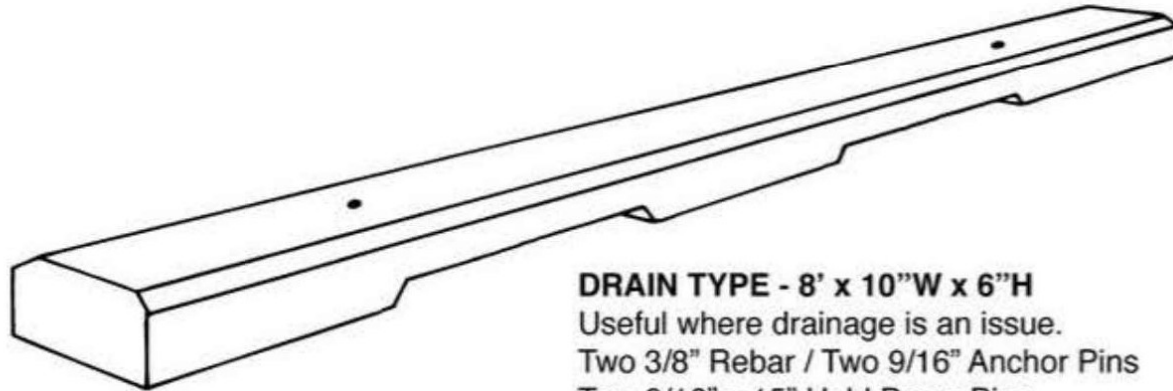




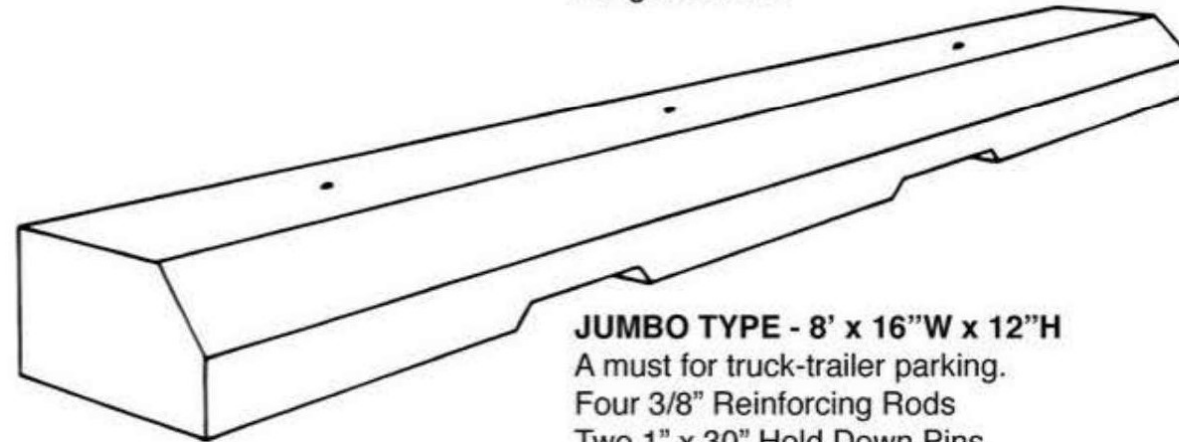
SOLID TYPE - 8' x 8"W x 6"H
 General All Purpose Bumper.
 Two 3/8" Reinforcing Rods
 Two 9/16" x 15" Hold Down Pins
 Weight 310 lbs.



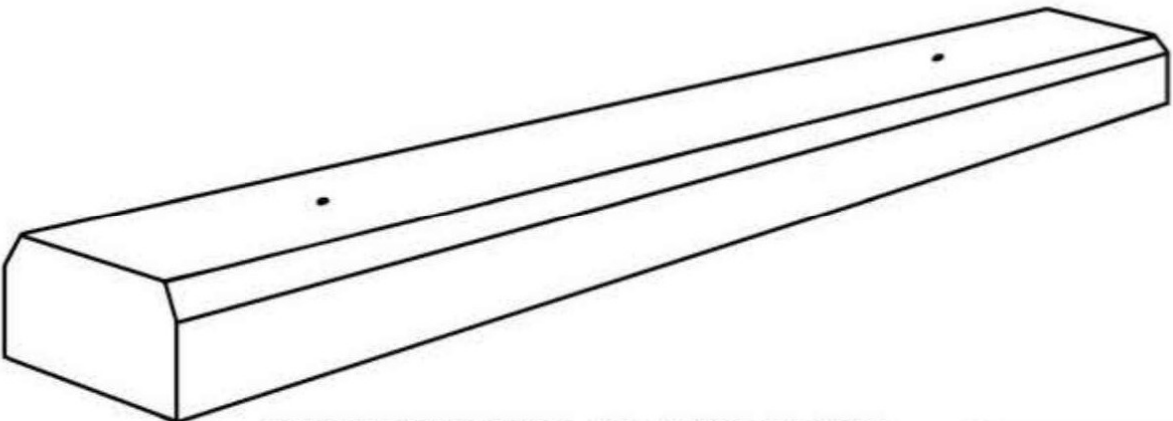
SUPER TYPE - 8' x 10"W x 9"H
 Popular for added protection with taller trucks and SUVs.
 Three 3/8" Reinforcing Rods
 Two 9/16" x 24" Hold Down Pins
 Weight 500 lbs.



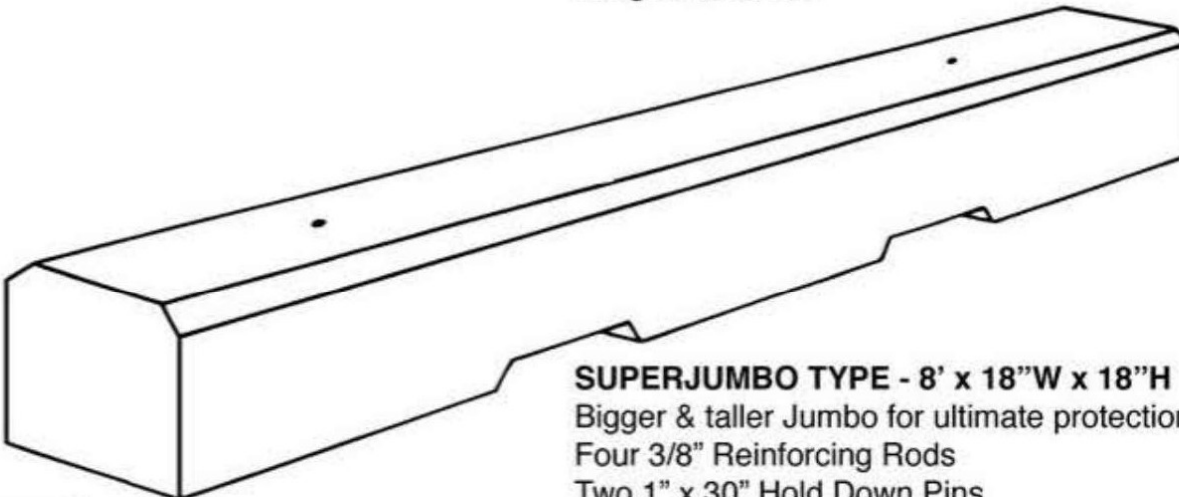
DRAIN TYPE - 8' x 10"W x 6"H
 Useful where drainage is an issue.
 Two 3/8" Rebar / Two 9/16" Anchor Pins
 Two 9/16" x 15" Hold Down Pins
 Weight 315 lbs.



JUMBO TYPE - 8' x 16"W x 12"H
 A must for truck-trailer parking.
 Four 3/8" Reinforcing Rods
 Two 1" x 30" Hold Down Pins
 Weight 1,350 lbs.



SUPER SOLID TYPE - 8' x 12"W x 7 1/2"H
 Mid sized bumper for added protection.
 Three 3/8" Reinforcing Rods
 Two 24" Hold Down Pins
 Weight 600 lbs.



SUPERJUMBO TYPE - 8' x 18"W x 18"H
 Bigger & taller Jumbo for ultimate protection.
 Four 3/8" Reinforcing Rods
 Two 1" x 30" Hold Down Pins
 Weight 2000 lbs.

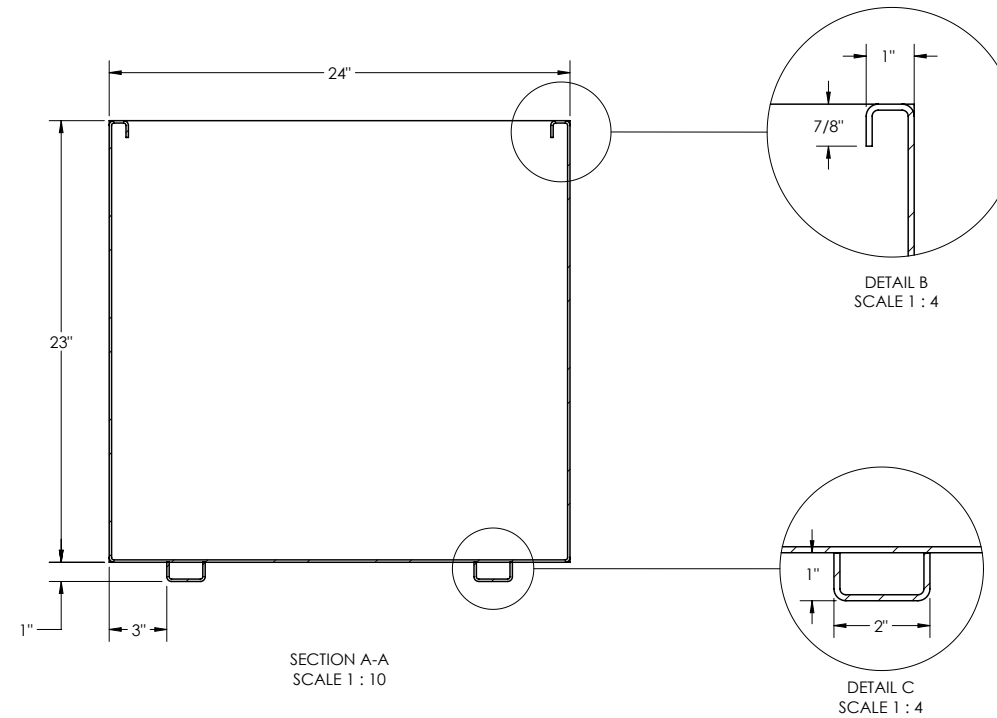
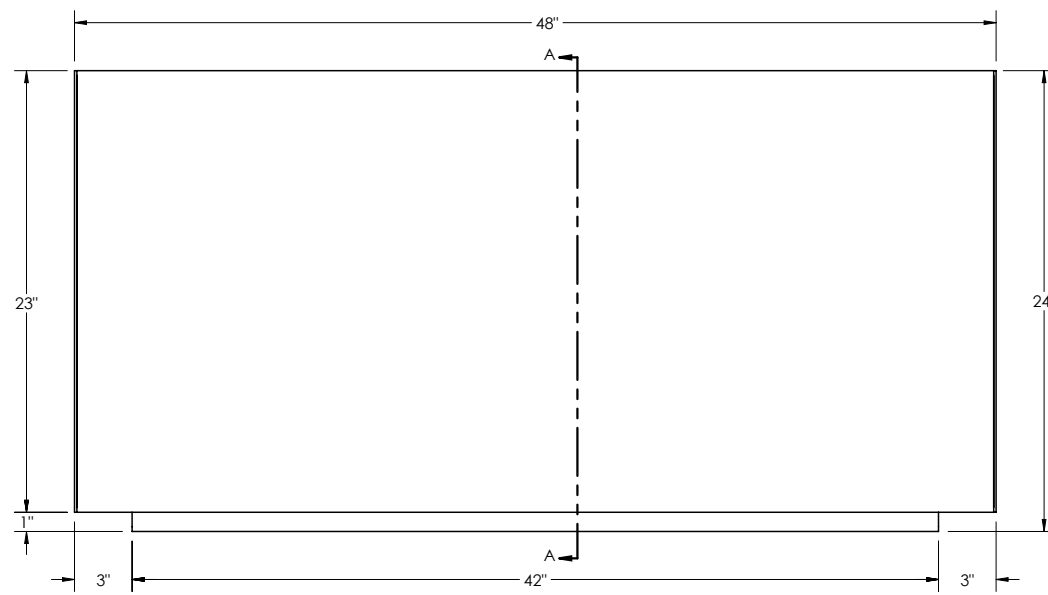
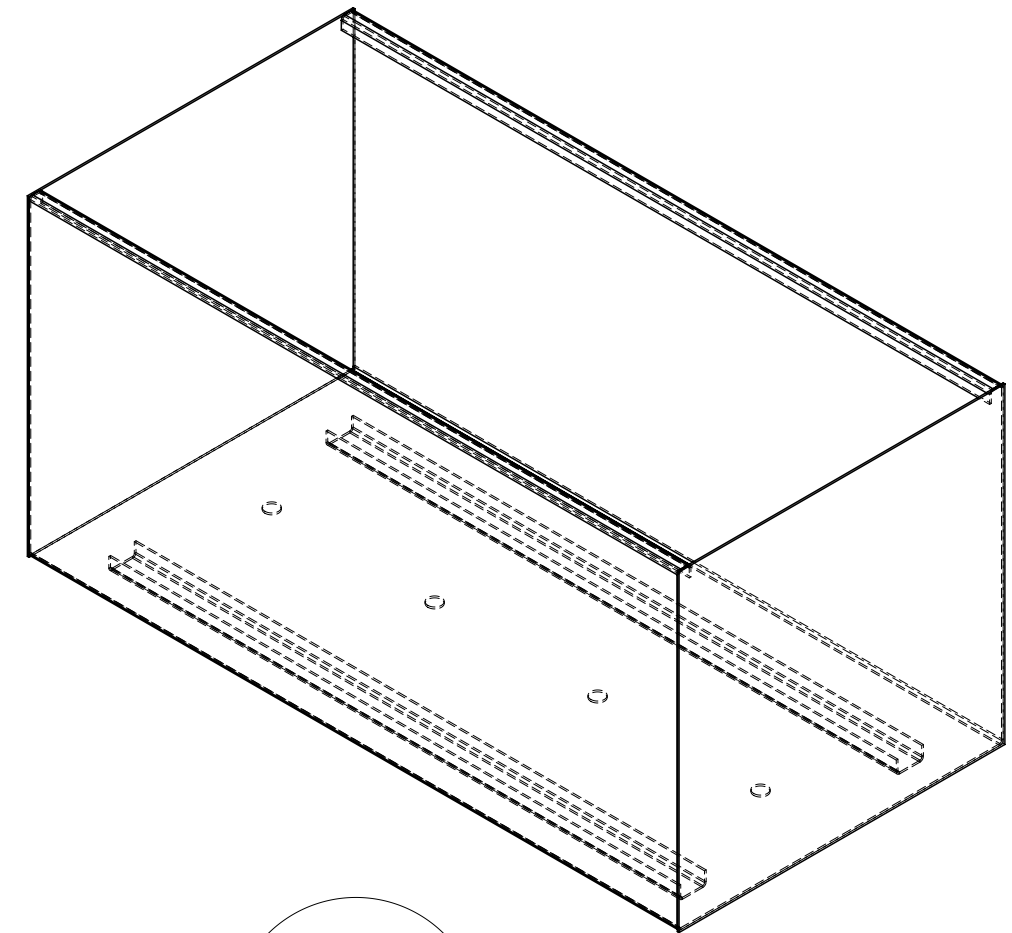
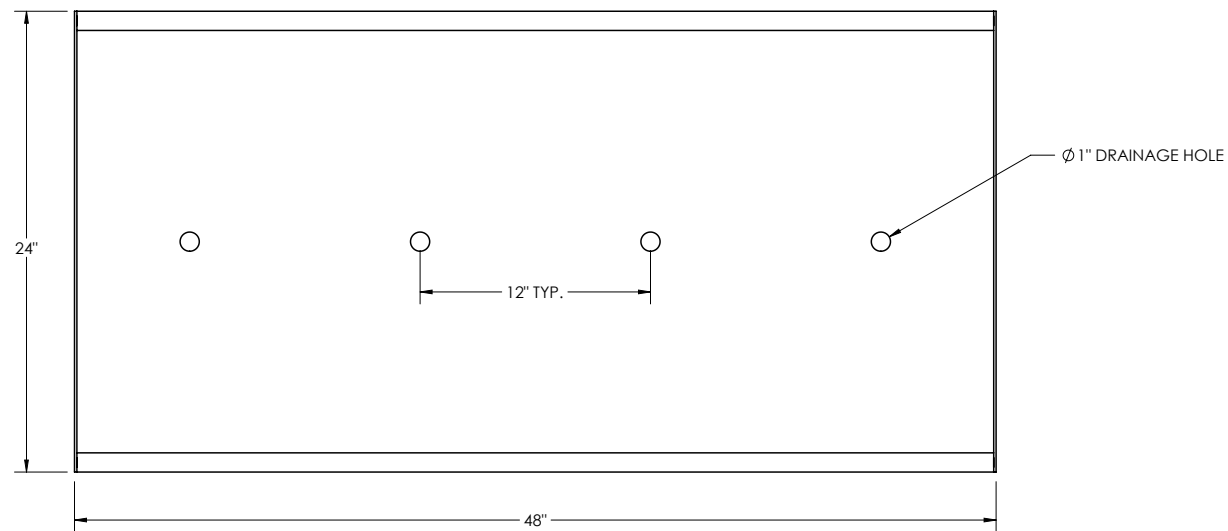
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 4,000 PSI Concrete
 All Types Available in
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 Special Sizes Upon Request




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www.americaneagleprecast.com



- NOTE:**
1. MATERIALS: CORTEN STEEL 1/8" THICK.
 2. QUANTITY: 1

| | | | | | | | | |
|--|--|---|-----------|------|--------------------------------|----------------------|-----------|--------------|
| PROPRIETARY AND CONFIDENTIAL | |  | NAME | DATE | TITLE: PLANTER 48L X 24W X 24H | | | |
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